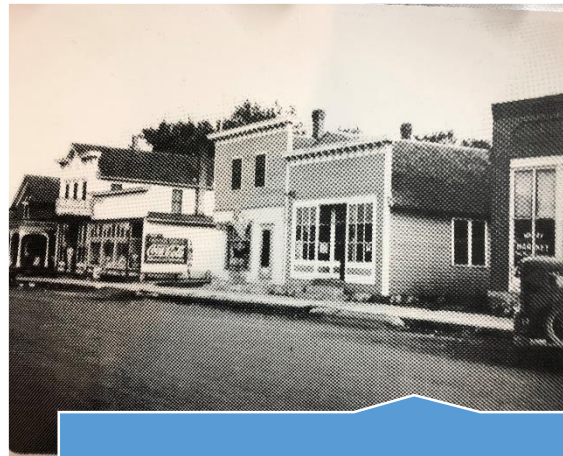


City of Norwood Young America



Historic Downtown
Norwood



Preserving History



Historic Downtown
Young America

Downtown Redevelopment Plan

Historic Downtown Norwood & Historic Downtown Young America

December, 2018

Table of Contents

Topic	Page(s)
Executive Summary	3-4
Planning Process	5-7
History of the Downtowns	8-12
District Boundaries	13
Historic Downtown Young America	14
Historic Downtown Norwood	15
Connectivity	16
Financial Incentives	17
Maintenance	18
Target Marketing-Infill	19
Tourism and Events	20
Implementation Plan	21-22
Exhibit A – Parcel Lists	23-31
Exhibit B – Design Guidelines	32
Exhibit C – Sample Historic Property Form	33

Executive Summary

Vision

The vision of the city of Norwood Young America is to build upon the history of the two downtown areas, by enhancing the unique characteristics and architectural features of buildings, resulting in vibrant small town downtowns with commercial, residential and governmental uses.

Plan Purpose

The purpose of the Downtown Redevelopment Plan is to establish the framework to accomplish the vision, identifying short and long range implementation tools.

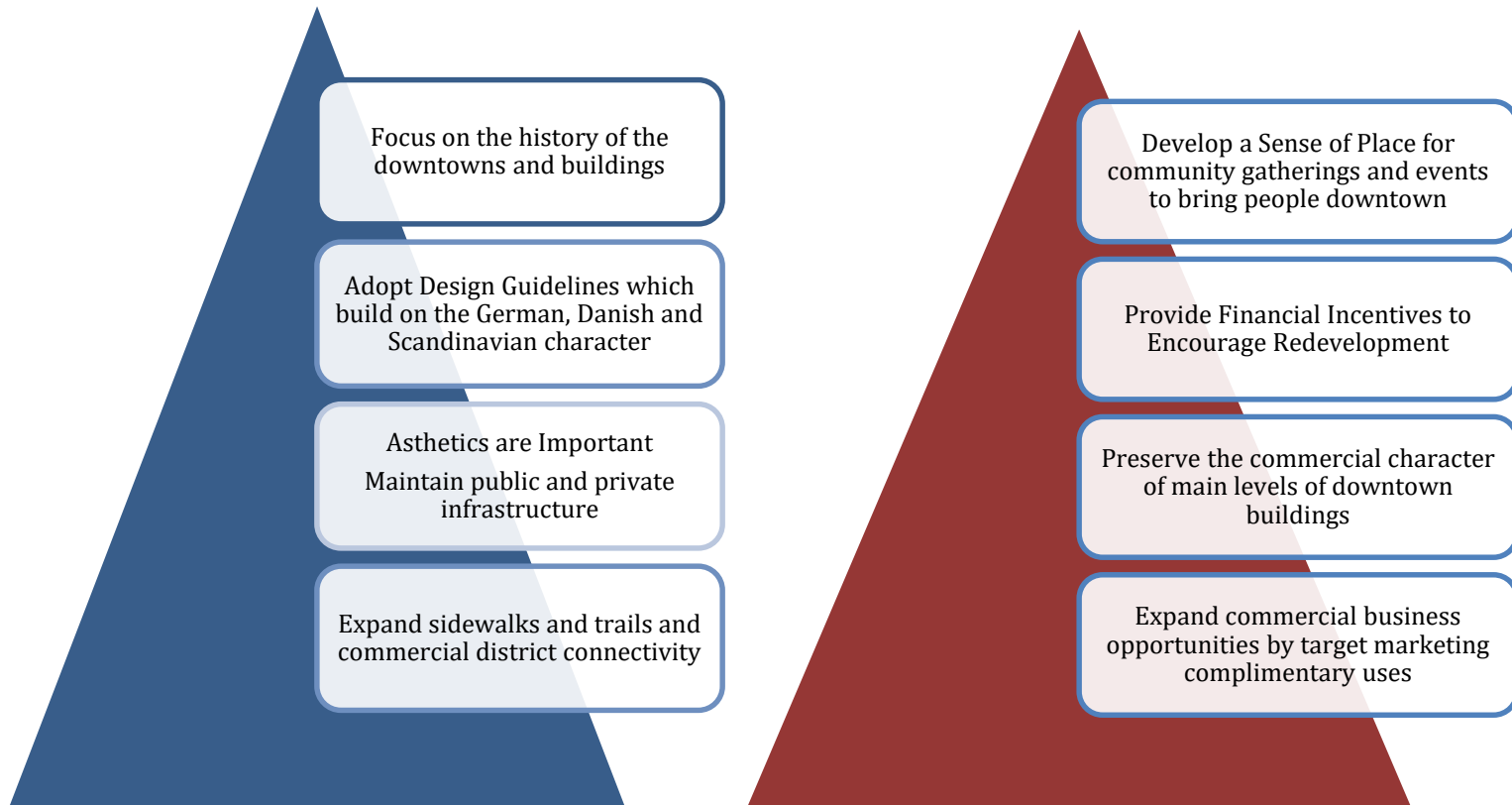
Planning Context

The downtown commercial districts have been identified by the City as important commercial centers for retail, services and community gathering. While the Highway 212 commercial district plays an important role in commerce, this Plan focuses on the two downtown districts. The downtowns include a mix of government uses (Historic Downtown Norwood), services, retail businesses, residential units with adjacent recreational opportunities. This Plan provides the framework for redevelopment of the downtowns within Norwood Young America. It provides the history and information on the parcels as well as establishes goals and an implementation plan for achieving the vision. Recognizing the importance of each commercial district, this Plan builds on the history of each downtown; designating the areas as “Historic Downtown Norwood” and “Historic Downtown Young America” and identifying the boundaries of each downtown.

Executive Summary



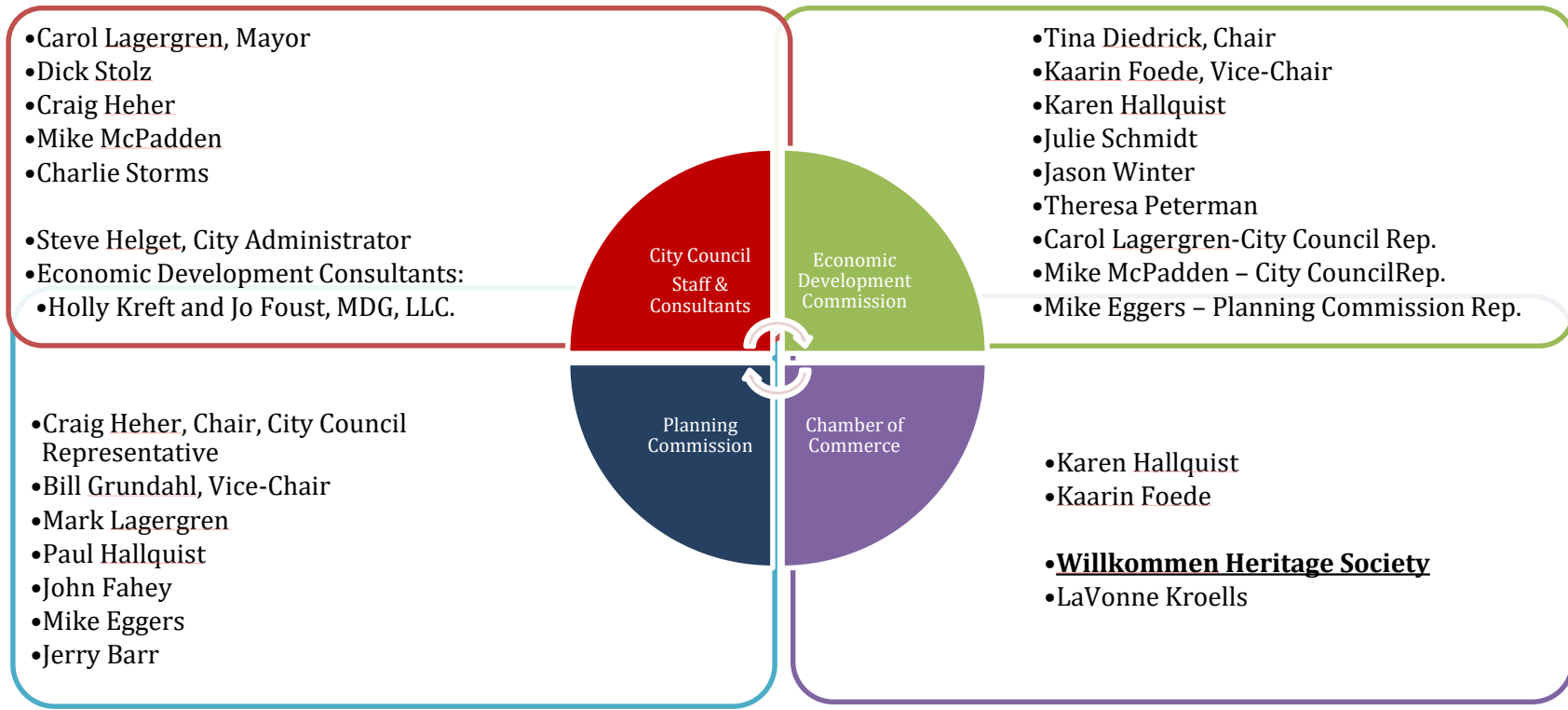
Focus Areas. Eight key focus areas resulted through the planning process. These are summarized below:



Planning Process



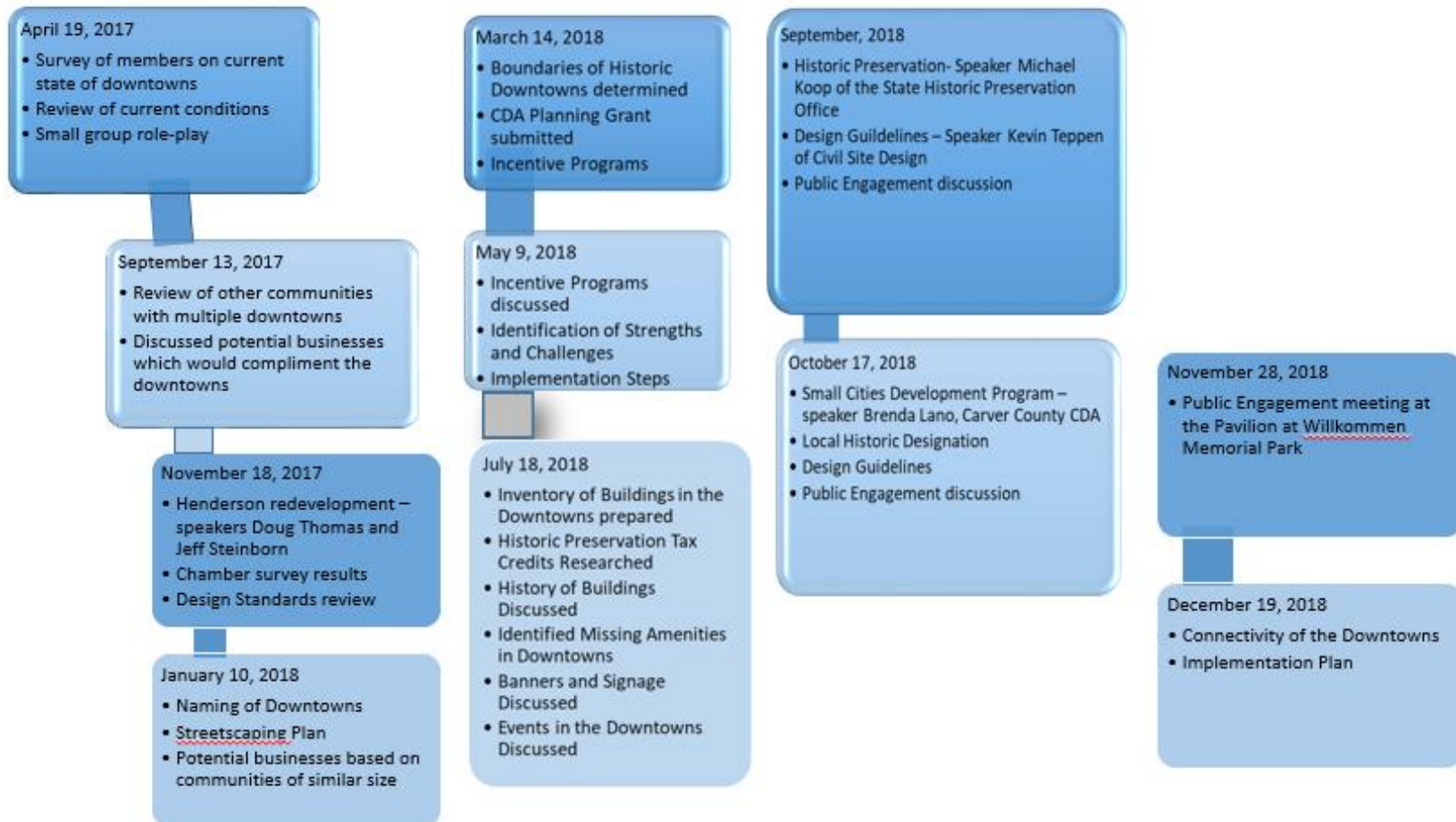
As a part of the planning process a group of elected and appointed officials from the City Council, Economic Development Commission, Planning Commission, Chamber of Commerce and Willkommen Heritage Society began meeting in April of 2017 to discuss the redevelopment of the downtowns. Guest speakers and consultants also attended meeting and presented information on historic districts, design guidelines and the Small Cities Development Grant Program. A grant was secured from the Carver County CDA to assist with the development of the Redevelopment Plan and Design Guidelines. Members participating in the planning sessions included:



Planning Process



Process. As a part of the planning process, eleven meetings were held over a 20-month period. A summary of topics discussed follows:



Planning Process

A public engagement session was held November 28th, with all downtown building and business owners invited. Approximately 20 people attended. The group consensus was:

1. Support for sharing the history of buildings with the business/property owners and encouraging redevelopment consistent with the time era in which the building was constructed.
2. Support for the application for SCDP grant and tying funding to design guidelines
3. Adoption of Design Guidelines
4. Support to increase maintenance in the downtowns including weeds, unused furniture, inoperable vehicles, streetlights, municipal parking lot, covered windows, and street trees. This may include review of current ordinances and amendments to reduce timeframes for compliance
5. Support for increased events in the downtowns, including food trucks

Joint Sessions



History



History of the Downtowns

The city of Norwood Young America had a unique beginning, two small towns in the mid 1800's with only a mile separating them. Young America was founded first, in 1856 by James Slocum Jr. and Dr. R.M. Kennedy. Slocum came to St. Paul, MN in 1855; from there he took a steamboat to Carver and then following Indian trails to what we know today as Young America. Early businesses were a saw and grist mill, general mercantile, and grain businesses.

Slocum's first choice of a name was Young America. In 1879 it was changed to Teuteberg. They later adopted the name Young America. By 1880 the population of Young America was 151.

About that time, the Milwaukee Railroad was going to install tracks near Young America. Residents didn't want to pay a bonus to have tracks laid, so the rail line moved a mile south. The original depot, built in 1872, was called Young America Station. Sensing a business opportunity Slocum and several townspeople re-established themselves near Young America Station.

As time passed Young America Station was incorporated in 1874 through special state legislation. Young America Station was renamed Norwood, after a friend and early settler of Slocum.

Slocum erected many buildings; a store, grain elevator, and the Bank of Norwood. Slocum was also involved in building the Methodist Church (Church of the Maples) in 1876. He also was the postmaster for both cities. In 1888, he relocated to Minneapolis.



Historic Downtown Young America

[Source: Carver County Historical Society]

History

On January 1, 1997 the two cities of Norwood and Young America merged into one city; Norwood Young America.

Historic Downtown Norwood contains approximately 3.61 acres of land, with a mix of residential units, public uses and commercial businesses. The 2018 assessed market value of the buildings and lots in the downtown is \$3,930,800. The buildings are primarily one and two-stories in height, with the exception of the city hall/senior housing complex on Elm Street, which is four stories high. Roof types are predominantly gable with a few flat roofs. An inventory of the parcels within Historic Downtown Norwood is included as Exhibit A. Below are photos of the historic downtown Norwood and current day downtown Norwood.

Historic Downtown Norwood



Source: Willkommen Heritage Society



Source: Google Maps

Historic Downtown Young America contains 42 parcels consisting of approximately 9.89 acres of land. The 2018 assessed market value of the buildings and lots in the downtown is \$6,121,000. The buildings are primarily one and two-stories in height. Roof types are predominantly gable and flat roofs. An inventory of the parcels within Historic Downtown Young America is included as Exhibit A. Following are photos of the historic downtown Young America and current day downtown Young America.

History



Source: Willkommen Heritage Society



Source: Google Maps



Goals identified:

Develop a historic walking tours

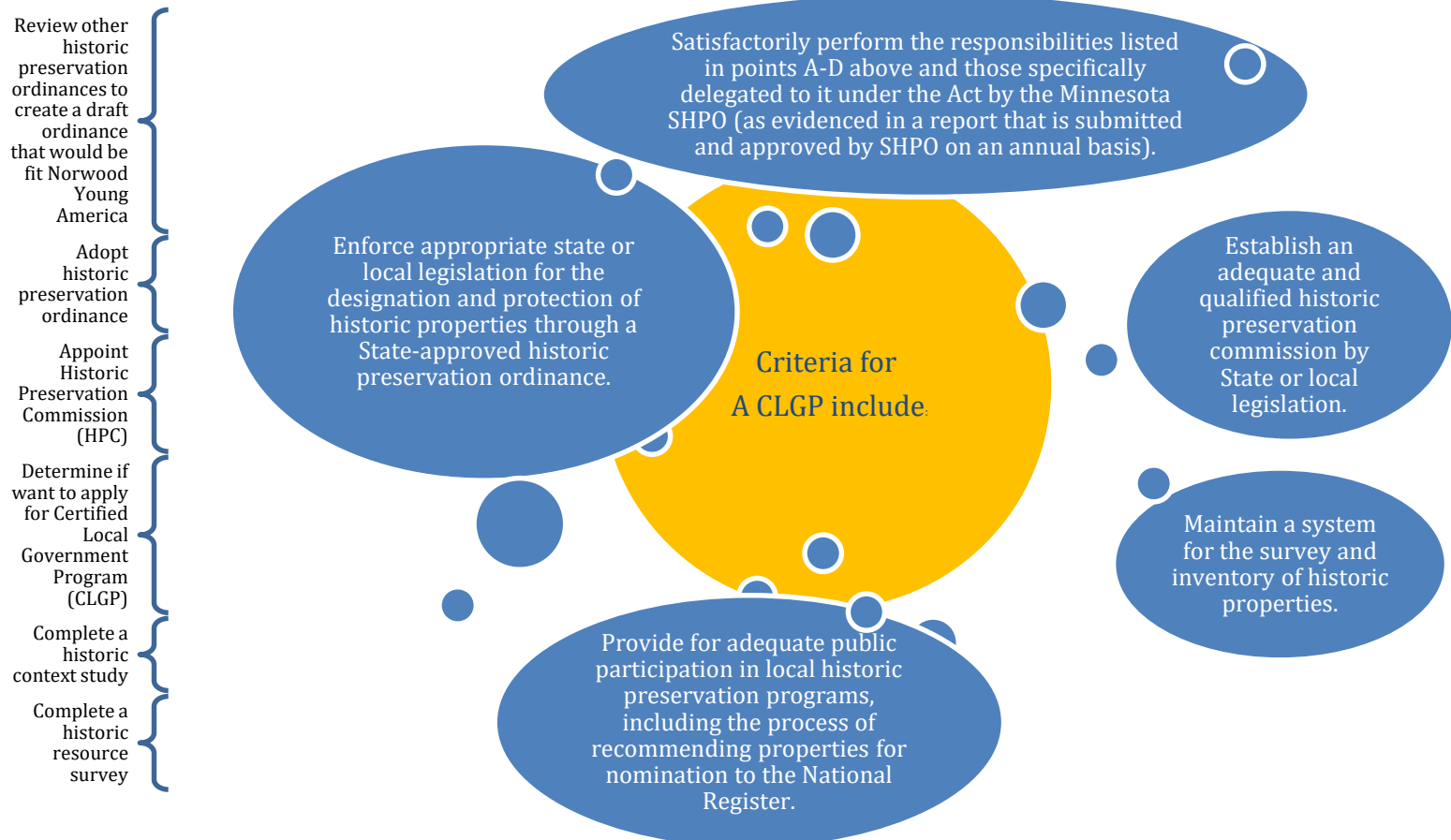
Provide building and business owners with information on the historic use(s) of buildings which could be displayed in buildings.

History



Historic Designations – State Historic Preservation Office (SHPO)

The Joint Committee discussed the potential creation of a Local Historic Preservation Ordinance and local designation. The committee found the information beneficial, but did not identify the development of a Local Historic Preservation Ordinance as a short-term goal. If the City were to choose to move forward with the creation of locally designated historic districts the implementation steps would be as follows:



History

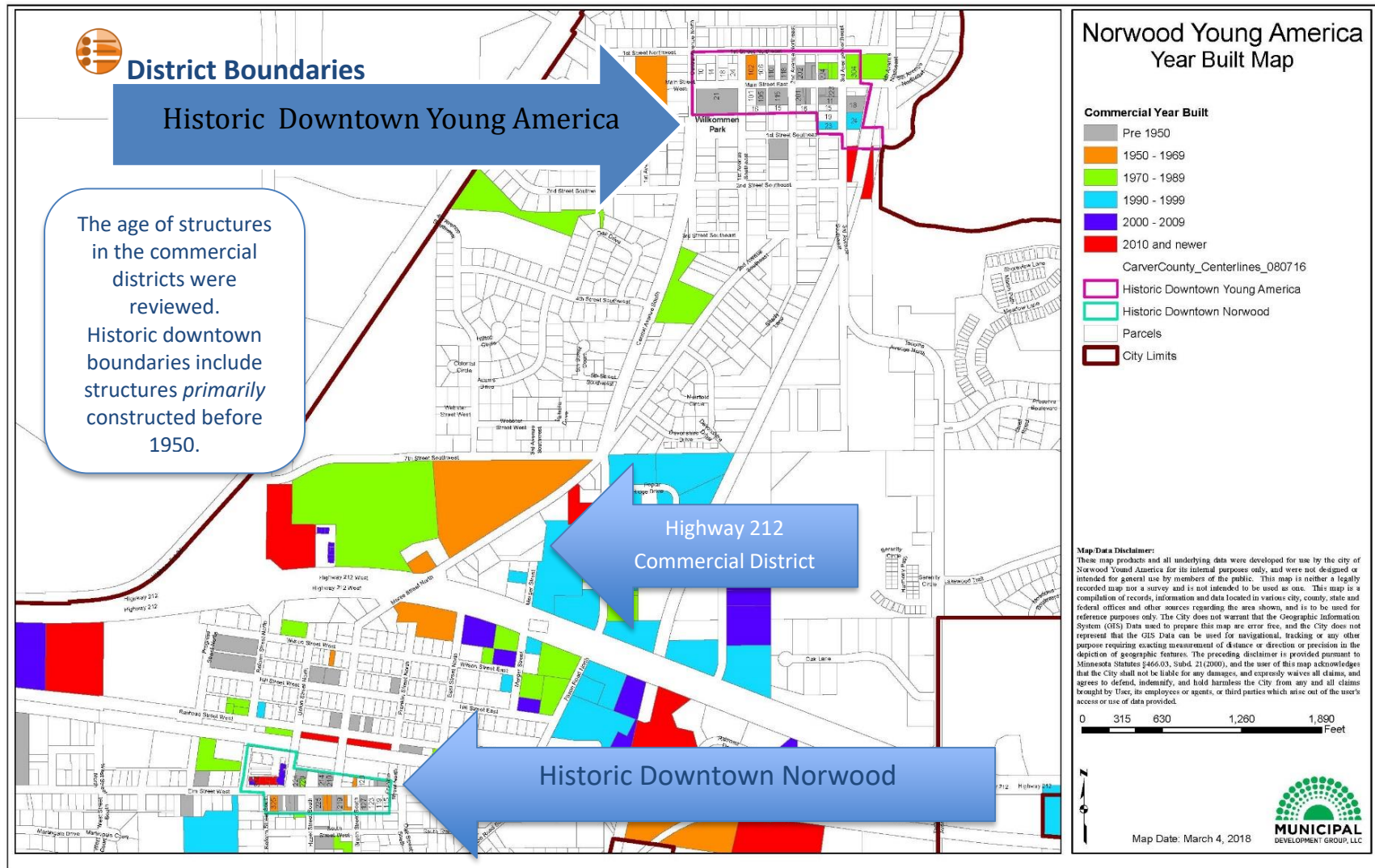
Grant funds are only available to CLGP communities. Annual matching grants are available to communities for projects such as:

- Historic resource surveys
- Preparation of local/national designation forms
- Historic preservation plans
- Building reuse studies
- Cultural landscape inventories
- Design guidelines for property owners
- Public education

Capital (brick and mortar) projects are not eligible. For 2018 there was \$91,000 available statewide.

Tax credits are available on a State and Federal level (up to 40%) for properties that are certified historic structures: either a building listed on the National Register of Historic Places or certified as contributing to a Registered Historic District.

District Boundaries



Historic Downtown Young America



Strengths

- Old brick buildings
- Church
- Wilkommen Park
- Historic Center
- Restaurants
- Mill Building
- Grovelli House
- Wide sidewalks
- Mature trees
- Evening lighting
- Baseball field
- Old Town
- Old City Hall

Opportunities

- Mix of commercial and residential
- Fill vacant buildings
- Redevelop and enhance older buildings
- Masonic Lodge
- Develop continuity in uses
- Develop an anchor
- Add parking
- Sidewalk improvements
- Add signage

Source: Google Earth

Historic Downtown Norwood



Strengths

- Sidewalks
- Trees
- Harms building is on the National Register of Historic Places
- Library/Oak Grove Senior Center
- The Quilting Grounds
- The Fire Department
- Old buildings
- Service businesses
- Nice long layout of the commercial district
- Visually easier access from Highway 212
- Sports complex
- More parking options
- Looks like a downtown

Opportunities

- Residential areas
- Fill vacant buildings
- Mark parking spaces
- Replace missing trees
- Add signs to unidentified/unsigned businesses
- Expand sidewalks/trails for connectivity to other areas
- Hayloft hole

Source: Google Earth

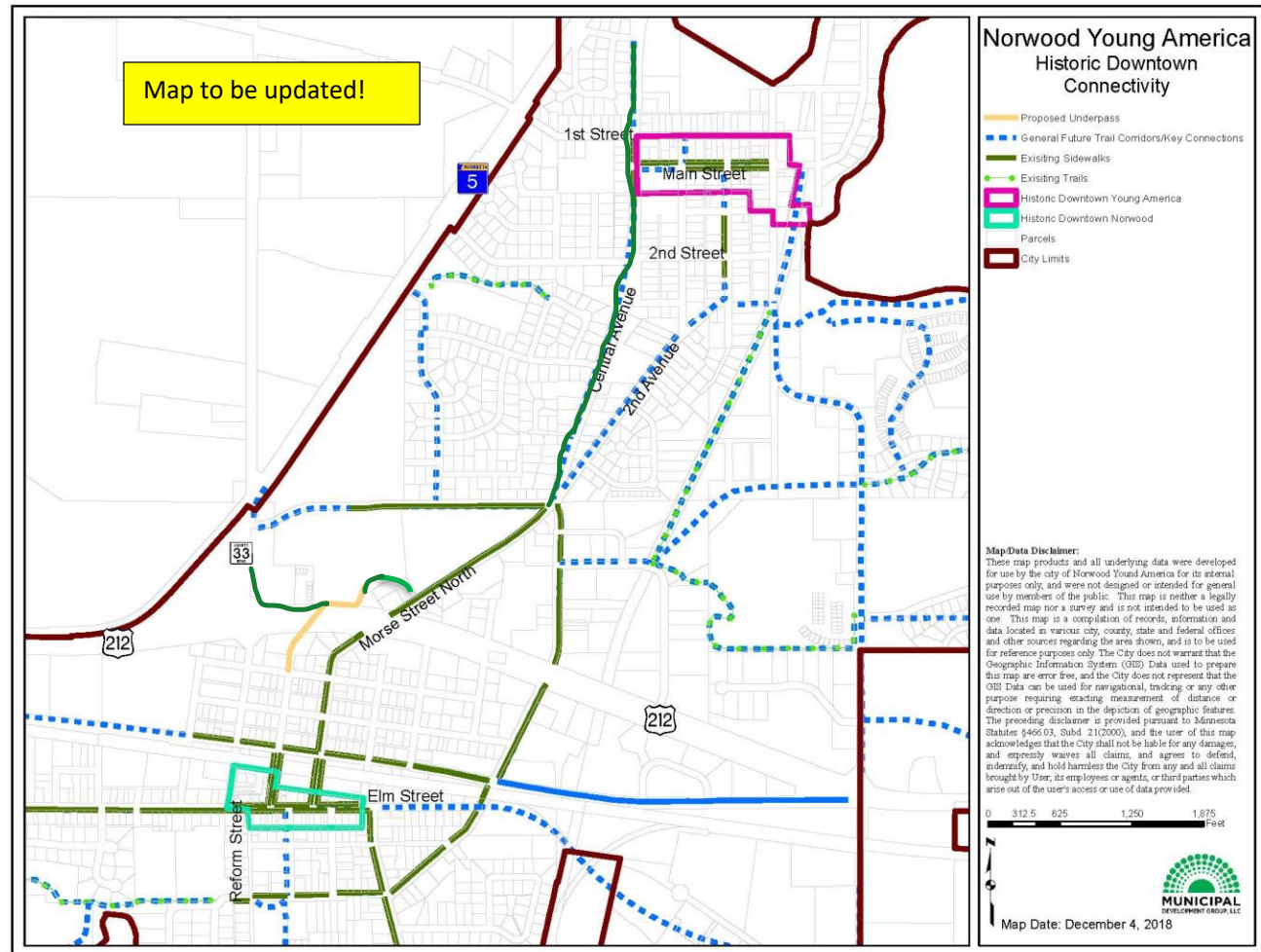
Connectivity



Connectivity

Downtown districts are not only accessed by vehicular traffic, but also provide for a pedestrian experience. Providing connections to neighborhoods, schools, parks and government facilities is important. The map to the right illustrates existing sidewalk and trail connections as well as proposed future trail connections which would link the two historic downtowns.

The future Highway 212 underpass plays an important role in the connectivity of the commercial districts to schools and parks.



Financial Incentives



Financial Incentives and Programs

In order to encourage and assist building and business owners with redevelopment projects, the committee discussed financial programs which could be offered. The following programs were identified:

Existing financial incentives which are available to assist with (re)development projects in the downtowns follow. These programs do not require any additional budget requirements. These include:

- MCCD and EDC loan programs. The EDC has \$135,000 in its Revolving Loan Fund, which the MCCD administers the loan process after review. The MCCD loan program has separate funding.
- Tax abatement and tax increment financing. The amount of assistance available is dependent on the size of the project and impact on the tax base.
- State programs to assist with contamination clean-up, and
- Local conventional loans and SBA loans through financial institutions.

New programs which would could be offered and would require the City/EDC budget funds include:

- A No Interest Loan program, (i.e. \$20,000 for four loans up to \$5,000 or two \$10,000 loans).
- An interest rate reduction program with local banks. We have contacted local banks to obtain input on their interest in such a program, as well as the dollar amount they would recommend.
- Tourism mini grants, (i.e. three mini grants of \$1000 each or \$3,000).

Grant programs which could be pursued include:

- The Small Cities Development Grant program (could be structured with a forgivable loan)

Fee Reduction Incentives. The following programs would not require the City to budget funds but would result in less revenue being collected by the City; and therefore would need to be noted in the recommendation to the City Council. These may include:

- A reduction in building permit fees (i.e. a 10% reduction up to \$500 for the first five downtown permits issued: Total cost \$2,500 *if* five permits are pulled).
- A reduction in water and sewer rates or connection fees if they “go green”. (i.e. \$500 reduction for first 5 permits or \$2,500 *if* projects occur).

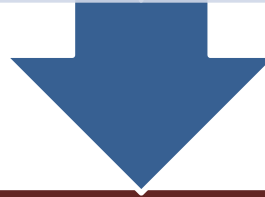
Maintenance & Public Infrastructure



Maintenance and Public Infrastructure Improvements. Two members of the Joint Committee toured the downtowns and noted several areas in which maintenance was recommended. At the public engagement session, downtown business representatives also expressed a desire for the City to invest in maintenance, code enforcement and improvements to public infrastructure. Examples follow:

Historic Downtown Norwood (Elm Street)

Weeds, grass needs to be cut.	Bare lot next to City Wide Wood Floors, vehicles, weeds, needs to be maintained better.	Old Flower shop/Meat market building outside area needs to be cleaned up.	Pavers – need to be lifted – to match curb level.	Weeds need to be maintained around tree metal grates.	Bottom of light poles need to be touched up, repainted.	Two trees have been removed on Elm Street and area has been cemented over.	Add more garbage cans near the BB park entrance, Pour House bar.
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Historic Downtown Young America (Main Street)

Weeds around Pavilion, weeds in sidewalks.	Missing four metal grates around various trees.	Fire Hydrants and surrounding pavers are popping up.	Pavers also sinking below curb line.	Pavers around gazebo need touch up.	Gazebo and pavilion needs painting	Yellow paint curbing needs to be touched up.	Mature trees, have no metal grates.	Old Post Office area is terrible.	Building across the street terrible shape.
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Target Marketing to Fill Vacancies



Target Marketed to Fill Vacant Spaces

Vacant spaces within the downtowns will fluctuate as buildings are leased or sold. Currently there are five vacant buildings in each historic downtown. Building owners were contacted and offered to market their space on the City website. The Joint Committee identified a desire to have niche (or destination) businesses located in the historic downtowns. Potential businesses identified included:



Tourism & Community Events



Tourism & Events

The Joint Committee discussed the current events hosted by the NYA Area Chamber of Commerce that bring people to the downtowns including:

Current Events

- Chili Cook off
- Taste of NYA
- Stiftungsfest
- Bean Bag Tournaments



Potential Events

- Downtown arts fair
- Youth dance studio performances
- Local authors
- Music, food
- Farmers Market
- Car cruise

Implementation Plan



Implementation Plan

Goals identified as part of the Redevelopment Plan include both short-term and long-term projects. Short-term projects could take place in the next 2-3 years. Long-term projects may take 5-10 years to implement. The joint committee may begin work on both short-term and long-term projects, but recognizing that long-term projects will take longer to implement and may require approval from other jurisdictions.

Short-term Projects

Category	Project	Responsible Party	Next Steps
Design /Aesthetics	Adopt Design Guidelines	EDC/Planning Commission/ City Council	1. EDC and Planning Commission Recommendation to adopt the Design Guidelines, as recommended as a part of the Redevelopment Plan 2. City Council adoption of Design Guidelines 3. Education of Guidelines to building and business owners
Historic Designation	Inventory Buildings	Willkommen Center/City	4. Use Property Template to inventory information about buildings 5. Share information with building owners
Vacant Spaces	Window Displays	EDC/Chamber	1. Contact building owners (vacant and storage) about window displays 2. Work with Willkommen Center and School District to create historic window displays
Maintenance	Add Benches	EDC/Public Works	1. Inventory existing benches 2. Determine location for new benches 3. Order benches 4. Place benches in identified locations
Maintenance	Add Fence or Hedge to Municipal Parking Lot	City/Public Works	1. Determine best solution (fence or hedge) 2. Install fence or hedge

Implementation Plan

Maintenance	Review Enforcement Procedure	Planning Commission/City Council	<ol style="list-style-type: none"> 1. Determine if timeframe for compliance should be shortened 2. Amend ordinance (if needed) 3. Review outdoor storage ordinances including parking of trailers.
Financial Incentives	Business Input	EDC/Chamber	<ol style="list-style-type: none"> 1. Host a joint meeting with the Chamber to gain greater input from businesses on financial incentives 2. Continue to pursue a Small Cities Development Grant
Tourism and Events	Downtown Arts Fair/Music	EDC/Chamber	<ol style="list-style-type: none"> 1. Meet with members of Arts Consortium of Carver County regarding festival in western Carver County 2. Reach out to local artists and dance theatres about interest
Connectivity	Wayfinding Signage	EDC	<ol style="list-style-type: none"> 1. Work with MNDOT staff on signage changes and additions 2. Work with MNDOT to have signage installed
Maintenance	Street Trees	City/Public Works	<ol style="list-style-type: none"> 1. Identify locations of where trees were removed 2. Determine if trees can be replaced in those locations 3. Have concrete removed and soil remediated 4. Plant trees

Long-term Projects

Category	Project	Responsible Party	Next Steps
Maintenance	Decorative Streetlights	City	<ol style="list-style-type: none"> 1. Identify locations for additional decorative streetlights in Historic Downtown Young America 2. Obtain costs for streetlights 3. Include in subsequent year budget 4. Have lights installed
Connectivity	Trails/Sidewalk	City	<ol style="list-style-type: none"> 1. Identify gaps in connectivity between existing trails and sidewalks 2. Budget for new trails or sidewalks in subsequent years 3. Research application for DNR Local Connections grant to help offset costs

Exhibit A – Downtown Parcel Inventory



Inventory of Parcels - Historic Downtown Young America

PID	Address	Year Built	Total Valuation 2018	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.670.0200	310 Elm Street West	Not Available			Brick, Block, Vinyl	Gable	4	Yes	Municipal	City Hall/Senior Housing
58.050.0620	122 Union Street North	Not Available	\$205,400	0.11	Wood	Gable	2	No	Multi-family residential	Apartments
58.050.0610	116 Union Street North	Not Available	\$134,400	0.03	Brick	Gable	2	No	Commercial/Residential	Salon and Apartment
58.050.0560	224 Elm Street West	Not Available	\$118,200	0.06	Brick	Gable	2	Yes	Commercial	Quilt Shop and Apartment
58.050.0570	220 Elm Street West	Not Available	\$170,000	0.06	Vinyl	Flat	1	Yes	Commercial/Residential	Salon and Apartments
58.050.0580	Not Available	Vacant Land	\$18,400	0.26	N/A	N/A	N/A	Yes	Vacant - Municipal	Parking Lot
58.050.0590	214 Elm Street West	Not Available	\$238,600	0.14	Brick and Block	Gable	1.5	Yes	Commercial	Accountant Office
58.050.0600	210 Elm Street West	Not Available	\$151,200	0.12	Brick, Wood, Vinyl	Gable and Flat	2	No	Commercial/Residential	Vacant and Apartments
58.050.0550	116 Morse Street N	1890	\$96,600	0.09	Wood	Gable	1	Yes	Single Family Residential	House

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation 2018	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.050.0540	110 Morse Street N	1880	\$123,900	0.07	Vinyl	Gable	2	No	Single Family Residential	Apartments
58.050.0500	124 Elm Street West	Not Available	\$55,900	0.05	Block and Wood	Gable	1.5	Yes	Commercial	Insurance Office
58.050.0490	120 Elm Street West	1890	\$99,700	0.15	Wood	Gable	2	Yes	Single Family Residential	House
58.050.0530	Not Available	Vacant Land	\$12,400	0.04	N/A	N/A	N/A	No	Vacant - Residential	Vacant Land
58.050.0510	114 Elm Street West	Not Available	\$59,100	0.02	Brick, Block, Vinyl	Gable	1.5	Yes	Commercial	Unknown
58.050.0520	110 Elm Street West	Not Available	\$250,600	0.07	Brick and Wood	Flat	2	No	Multi-family residential	Apartments
58.050.0080	115 Elm Street West	1885	\$124,900	0.17	Vinyl	Gable	2	Yes	Single Family Residential	House
58.050.0090	119 Elm Street West	1885	\$96,600	0.17	Wood	Gable	1.5	Yes	Single Family Residential	House
58.050.0100	123 Elm Street West	1890	\$128,700	0.17	Vinyl	Gable	2	No	Multi-family residential	Apartments
58.050.0110	127 Elm Street West	Not Available	\$126,500	0.17	Stucco	Gable	1.5	No	Commercial	Unknown

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation 2018	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.050.0180	211 Elm Street West	1900	\$114,900	0.07	Vinyl	Gable	2	Yes	Single Family Residential	House
58.050.0170	215 Elm Street West	Not Available	\$72,700	0.09	Brick and Wood	Flat	1	No	Commercial	Unknown
58.050.0190	219 Elm Street West	Not Available	\$225,700	0.17	Vinyl and Brick	Gable and Flat	2	No	Residential/Commercial	Apartments/Vacant
58.050.0200	221 Elm Street West	Not Available	\$94,100	0.11	Stucco and Brick	Gable	2	No	Commercial	Vacant
58.050.0210	223 Elm Street West	Not Available	\$57,900	0.08	Brick	Flat	1	No	Commercial	Vacant
58.050.0220	225 Elm Street West	Not Available	\$81,800	0.1	Brick	Flat	1	No	Commercial	Dog Groomer/Vacant
58.050.0230	227 Elm Street West	1890	\$170,000	0.21	Vinyl	Gable	2	Yes	Single Family Residential	House
58.050.0290	Not Available	Vacant Land	\$23,100	0.17	N/A	N/A	N/A	No	Commercial	Gravel Parking Lot
58.050.0300	315 Elm Street West	Not Available	\$49,800	0.06	Wood and Block	Flat	1.5	No	Commercial	Vacant
58.050.0310	319 Elm Street West	Not Available	\$119,700	0.1	Brick	Flat	2	No	Commercial/Residential	Apartments/Vacant

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.050.0380	321 Elm Street West	Not Available	\$117,900	0.09	Stucco and Brick	Flat	1	Yes	Industrial	Wood Flooring
58.050.0320	323 Elm Street West	Vacant Land	\$6,700	0.08	N/A	N/A	N/A	Yes	Commercial	Gravel Parking Lot
58.050.0330	325 Elm Street West	1900	\$166,900	0.1	Brick and Vinyl	Gable	2	Yes	Commercial/Residential	Bar/Apartment
58.050.0340	327 Elm Street West	Not Available	\$400,700	0.17	Brick	Flat	2	Yes	Municipal	Fire Station/Community Room
58.050.0350	327 Elm Street West	Vacant Land	\$17,800	0.06	N/A	N/A	N/A	Yes	Municipal	Parking Lot
Total			\$3,930,800	3.61						

Exhibit A – Downtown Parcel Inventory



Inventory of Parcels - Historic Downtown Young America

PID	Address	Year Built	Total Valuation	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.051.0270	7 1st Street NE	1951	\$117,400	0.23	Vinyl	Gable	1	No	Single family residential	House
58.051.0280	14 Main Street East	1938	\$156,400	0.23	Wood & stone	Gable	1.5	Yes	Single family residential	House
58.051.0290	10 Main Street East	1919	\$154,000	0.23	Wood	Gable	1.5	Yes	Single family residential	House
58.051.0300	24 Main Street East	1890	\$143,700	0.23	Wood	Gable	2	Yes	Single family residential	House
58.051.0310	18 Main Street East	1961	\$137,300	0.17	Wood	Gable	1	No	Single family residential	House
58.051.0320	17 1st Street NE	1981	\$149,800	0.17	Vinyl	Gable	1	Yes	Single family residential	House
58.051.0330	19 1st Street NE	1900	\$104,000	0.05	Wood & stone	Gable	1.5	Yes	Single family residential	House
58.051.0340	19 1st Street NE	Not Available	Not Available	0.05	Metal & Wood	Gable & Hoop	1	Yes	Single family residential	Storage
58.051.0350	102 Main Street East	Not Available	\$833,100	0.39	Brick	Flat & Gable	1	Yes	Institutional	Museum/food shelf
58.051.0360	106 Main Street East	1910	\$174,600	0.3	Vinyl	Gable	2	Yes	Single family residential	House

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.051.0370	110 Main Street East	1948	\$119,500	0.13	Stucco	Flat	1	Yes	Commercial and residential	Hair Salon/Apartment
58.051.0390	114 Main Street East	1880	\$157,100	0.22	Vinyl	Gable	2	Yes	Single family residential	House
58.051.0380	118 Main Street East	Not Available	\$122,900	0.16	Vinyl and Wood	Gable	1	Yes	Commercial	Bar
58.051.0460	202 Main Street East	Not Available	\$129,000	0.18	Wood	Gable	2	No	Multi-family residential	Apartment
58.051.0440	216 Main Street East	1881	\$126,000	0.17	Brick and Wood	Gable	2	No	Commercial and residential	Vacant
58.051.0450	220 Main Street East	Not Available	\$65,800	0.07	Metal, Brick and Wood	Gable	1	Yes	Commercial	Plumbing business storage
58.051.0470	224 Main Street East	Not Available	\$256,700	0.14	Vinyl	Gable and Flat	2	No	Multi-family residential	Apartment
58.051.0490	228 Main Street East	Not Available	\$48,100.00	0.1	Block and vinyl	Flat	1	Yes	Commercial	Laundromat
58.051.0500	232 Main Street East	Not Available	\$89,200.00	0.13	Metal	Gable	1	No	Commercial	Vacant

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.051.0530	304 Main Street East	Not Available	\$248,600	0.45	Brick	Gable	1	No	Commercial	Vacant
58.051.0570	315 Main Street East	1900	\$101,200	0.18	Wood	Gable	1.5	No	Single family residential	House
58.051.0580	10 3rd Avenue SE	1904	\$4,600	0.05	Brick	Flat	2	No	Single family residential	Vacant
58.051.0610	18 3rd Avenue SE	Not Available	\$343,900	0.42	Metal, Brick and Wood	Gable	2	No	Commercial	Home décor and antique store
58.051.0600	24 3rd Avenue SE	Not Available	\$188,500	0.35	Metal	Gable	1.5	Yes	Municipal	Public Works
58.076.0090	No Address	Vacant Land	\$36,600	0.46	N/A	N/A	N/A	Yes	Municipal	Public Works
58.051.0690	23 3rd Avenue SE	Not Available	\$317,800	0.23	Metal	Gable	1.5	Yes	Municipal	Fire Station
58.051.0700	19 3rd Avenue SE	1923	\$162,800	0.29	Wood	Gable	1.5	Yes	Single family residential	House
58.051.0710	15 3rd Avenue SE	1904	\$103,400	0.17	Vinyl	Gable	1.5	Yes	Single family residential	House
58.051.0720	11 3rd Avenue SE	Not Available	\$74,400	0.21	Metal and block	Gable	1	No	Industrial	Vacant

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.051.0730	223 Main Street East	1910	\$212,900	0.17	Brick, block, stucco	Flat and Gable	2	Yes	Commercial/ Residential	Bar and House
58.051.0740	217 Main Street East	Not Available	\$88,500	0.08	Wood	Gable	2	No	Commercial/ Residential	Apartment and Vacant
58.051.0640	213 Main Street East	1920	\$177,700	0.12	Wood	Gable	2	No	Multi-family residential	Apartment
58.051.0650	209 Main Street East	Not Available	\$75,800	0.1	Wood, brick, stucco	Flat and Gable	2	No	Commercial	Vacant
58.051.0660	16 2nd Avenue SE	1896	\$153,800	0.21	Wood	Gable	2	Yes	Single family residential	House
58.051.0630	205 Main Street East	Not Available	\$44,300	0.05	Brick	Flat	2	Yes	Single family residential	House
58.051.0620	201 Main Street East	Not Available	\$283,400	0.18	Stucco and wood	Gable	2	No	Multi-family residential	Apartment
58.051.0790	15 2nd Avenue SE	Not Available	\$35,800	0.17	Metal	Gable	1	Yes	Industrial	Vehicle repair and storage
58.051.0800	115 Main Street East	Not Available	\$301,300	0.47	Metal	Gable	1	Yes	Industrial	Vehicle repair and storage

Exhibit A – Downtown Parcel Inventory

PID	Address	Year Built	Total Valuation	Acreage	Building Material	Roof Type	Stories	Owner Occupied	Tax Class	Use
58.051.0760	105 Main Street East	Not Available	\$240,400	0.23	Wood	Gable and Flat	2	No	Commercial/Residential	Vacant
58.051.0750	101 Main Street East	1900	\$137,300	0.23	Wood	Gable	2	Yes	Single family residential	House
58.051.0770	16 1st Avenue SE	1953	\$148,100	0.23	Vinyl	Gable	1	Yes	Single family residential	House
58.051.0810	21 Main Street East	Not Available	\$518,500	1.49	Wood	Gable	1.5	Yes	Municipal	Willkommen Park
Total			\$6,121,600	9.89						

Exhibit B- Design Guidelines



Design Guidelines – Exhibit B includes Design Guidelines to build on the unique character of each downtown. The Guidelines include goals, proposed architectural features and suggested color palettes. They also include recommendations for infill and the addition of fencing.

At this time, these are Design Guidelines and not requirements. It is understood that some design recommendations may not apply to all properties. It may be more suitable to have renovations consistent with the era of the building construction or previous use than the historic character of the district.

Historic Downtown Young America is recommended to incorporate a stick-built German and/or Danish architecture style.

Historic Downtown Norwood is recommended to incorporate a Scandinavian and/or Danish architectural style.



110 EAST MAIN STREET



115 EAST MAIN STREET



102 EAST MAIN STREET



118 EAST MAIN STREET



114 EAST MAIN STREET



105 EAST MAIN STREET



232 EAST MAIN STREET



220 EAST MAIN STREET



21 EAST MAIN STREET



201 EAST MAIN STREET



217 EAST MAIN STREET



223 EAST MAIN STREET

EXISTING CONDITIONS

Historic Downtown Young America

- 1** Establish a rich and vibrant small-town downtown environment that encourages pedestrian and human activity.
- 2** Maintain an appropriate scale for downtown Young America that fits the existing character of East Main Street while including space for commerce, office, and residential uses.
- 3** Develop and create structures of similar architecture quality and style to enforce the quality and character of historic downtown Young America.
- 4** Use vegetation, street furniture lighting, and signage to create a distinct character for downtown Young America.



These goals are the foundation from which the following guidelines were established. The following guidelines should be thought of as objectives or implementation steps to achieving the goals for historic downtown Young America. These standards have been established to ensure a successful, high-quality downtown environment. The purpose of establishing Design Guidelines for Historic Downtown Young America is to ensure appropriate development and redevelopment activities occur and that the vision and goals for the downtown are achieved through appropriate architectural styles and details.

GOALS

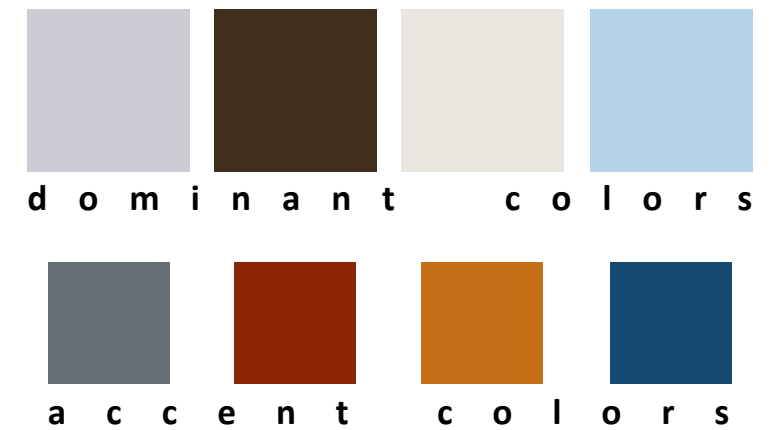
ARCHITECTURE



Through a site site inventory and analysis several parameters were set for general character, architecture and color for the historic downtown. The images above demonstrate the architectural style, color and massing that shall dominate the downtown streetscapes. The main goal is to use common elements (i.e. color) throughout downtown to unit the users and create a more uniform frontage. The following Guidelines shall apply to the historic downtown Young America.

Rich colors with muted tones should be used on structures throughout the historic downtown. Dominant colors on structures should be muted rich tones. For example, chocolate brown, burgundy, slate blue, and taupe. Non-dominant colors could be more diverse, but should not be more than 20-percent of a buildingfront. Primary colors may be used as accents provided they are consistent with and do not exceed 20-percent of the facade.

COLOR



ARCHITECTURAL ELEMENTS

ARCHITECTURE - ELEMENTS & CHARACTERISTICS

The sketch and pictures identify the appropriate style of architecture for the historic downtown of Young America. The downtown architecture should create a special and unique environment and should feel different than the surrounding environment.



The sketch and example pictures demonstrate a mix of architecture styles, including a neo-traditional structure as well as a stick-built traditional Danish structure. All images include the following characteristics that unite them and make them appropriate for Downtown:

- High percentage of glass at the street level makes them inviting and adds an element of transparency.
- Architectural details including the parapets, cornices, window casings and sashes, and roof lines all have clean lines.
- Clearly identified structures and signage affixed to the facade of the structure is required on all buildings in the Downtown District.



ARCHITECTURAL ELEMENTS

ARCHITECTURE

Objective: To create an environment with an architectural palette that is cohesive and unique to historic downtown Young America.

Individual storefronts and separation between structures (perceived or literal) should dominate the downtown. Mixed-use structures, when stacked, are appropriate for the downtown provided the scales does not exceed two and a half stories.

The stick-build German and/or Danish architectural style shall dominate the downtown. Neo-traditional structures are appropriate provided color and scale matches the downtown character. If architectural styles of adjacent structures are different, common elements such as window style or color should be used to help create a more cohesive street frontage.

Buildings and structures shall have a distinct and varying roof line to encourage interest and variety. Structures shall have a district entry to clearly delineate the entrance users.

The first level of all structures shall have a transparent facade that is interesting and inviting.

Four-sided architecture is required particularly for structures with double frontage (i.e. corner lot), or visibility from major thoroughfares.



ARCHITECTURAL ELEMENTS

MATERIALS

It is the objective of the City to use high-quality materials that are warm, welcoming and sturdy throughout the Downtown District. High-quality traditional materials for structures (wood, brick, stucco, hardie, stone, etc.) are required for all structures.

The following material breakdown is required for all structures:

Street Level (Calculation includes parapet and false fronts for single-story structures):

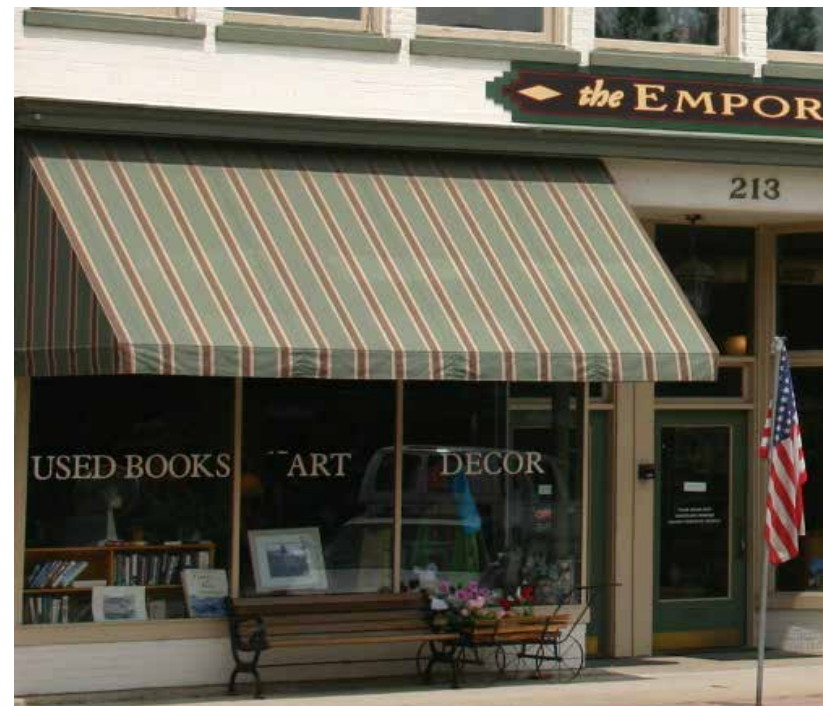
Fenestration/Windows:	60% Minimum
Wood/Brick/Stone/Hardie:	35% Minimum
EIFS/Aluminum/Other:	5% Maximum

2nd Level (Calculation includes parapets and false fronts):

Fenestration/Windows:	40% Minimum
Wood/Brick/Stone/Hardie:	50% Minimum
EIFS/Aluminum/Other:	10% Maximum

The residential scale should be compatible with the scale and massing of adjacent commercial and/or office uses. All residential areas are open to discussion for material breakdown recommendations.

Clearly marked, welcoming entrances shall be required for all retail and office space in the Downtown District. All doors shall have a minimum of 50% of their area transparent (i.e. glass).



ARCHITECTURAL ELEMENTS

SCALE & MASSING

Architectural style and quality will directly influence scale and massing because style and height restrictions will naturally control the size and location of a structure. Additional parameters as set forth below shall also guide development in Downtown.

There should be no more than half (½) story difference between structures on the street to create some visual consistency.

Parapets and false fronts may be used to achieve greater consistency along the street frontage.

Parapets and false fronts will be included in materials calculations on the façade. These areas must include architectural detail, and/or fenestration to add visual interest to a façade.

Building space can be literal or perceived. (See Image) Buildings may be connected, but the illusion of spacing must be created to eliminate the appearance of a strip mall or connected multi-use structures.

Architectural detailing and fenestration should be used to reduce large expanses of walls without interest.

Buildings shall be a minimum of 15-feet wide but should be no larger than 40-feet wide; each user must have a minimum of 15-feet of frontage at the street-level.

Structures shall be no taller than 2.5 stories (35-feet maximum), where the half story is attributed to a parapet, false front, or roof pitch.



The above images represent the existing and appropriate scale and massing for the historic downtown of Young America. Parapets, false fronts and facade treatment help to break up the mass of structures and to create a quaint downtown feel.

ARCHITECTURAL ELEMENTS

SIGNAGE

A sign package should be created that allows each user the creativity to clearly define their space, but to do so in a manner that creates a visually appealing street frontage. Clear communication, as well as an aesthetic that caters to the small-town flavor shall be required in all sign packages.

All proposed signage should meet the current sign ordinance. In addition to these recommendations, the following standards shall be applied to signs within the Downtown District.

Clean, elegant signage with small town character. Materials and scale should reflect the materials and scale of each user.

Supplemental signs are acceptable in this District, including sandwich boards and projecting signs.

Wall signage (i.e. signage affixed to the structure and flush with the façade) is required to ensure visibility of each user.

The following recommendations shall apply to the signage package for each user:

- **Area:** The total signage package shall not exceed 15-percent of the total façade area. This area includes the primary wall sign, supplemental signs including sandwich boards, projecting signs, and window signage.
- **Sign Materials:** Primary material should be wood. EIFS may also be included but will be calculated against materials recommendations. Other materials include metals such as aluminum or wrought iron.
- **Lighting:** All signs within the Downtown District must have secondary lighting. Internal lighting is not acceptable. This includes neon signs which are also not permitted. Backlit signs are appropriate provided they meet lighting standards set forth in the City's zoning code. Light pollution should be reduced when possible and energy reducing light fixtures used when possible.

Users should consider the scale and size of the sign package, and should develop a package that is complimentary to the user and the overall City character. A full signage package should be submitted for review during initial submittal of each project. This package will be reviewed by the Planning Commission or other design review commissions as identified by the City.



ARCHITECTURAL ELEMENTS

OFF-STREET PARKING INFILL

Off-street parking lot areas and vacant parcels shall be reduced and infilled wherever possible. Off-street parking lot widths shall be encouraged to reduce drive aisles to one per parking lot with two parking stall aisles on either side. Wherever an off-street parking lot or vacant parcel of land is to remain, permanent ornamental fencing and/or vertical landscaping shall be implemented at the front perimeter to create a clearly defined street frontage and pedestrian walkway in the downtown district.



Downtown Young America currently has implemented on-street parking and minimized off-street parking lots in the downtown district that are visible to the central street frontage. There are no current vacant parcels of land and/or off-street parking lots that must be reduced or infilled. These design guidelines shall serve as future development instructions for the downtown district.

ARCHITECTURAL ELEMENTS



122 NORTH UNION STREET



314 ELM STREET WEST



315 ELM STREET WEST



224 ELM STREET WEST



224 ELM STREET WEST



219 ELM STREET WEST



VACANT PARKING LOT ELM STREET W.



325 ELM STREET WEST



221 ELM STREET WEST



215 ELM STREET WEST



110 ELM STREET WEST



210 ELM STREET WEST

EXISTING CONDITIONS

Historic Downtown Norwood

- 1** Establish a rich and vibrant small-town downtown environment that encourages pedestrian and human activity.
- 2** Maintain an appropriate scale for downtown Norwood that fits the existing character of Elm Street West while including space for commerce, office, and residential uses.
- 3** Develop and create structures of similar architecture quality and style to enforce the quality and character of historic downtown Norwood.
- 4** Use vegetation, street furniture lighting, and signage to create a distinct character for downtown Norwood.



These goals are the foundation from which the following guidelines were established. The following guidelines should be thought of as objectives or implementation steps to achieving the goals for historic downtown Young America. These standards have been established to ensure a successful, high-quality downtown environment. The purpose of establishing Design Guidelines for Historic Downtown Norwood is to ensure appropriate development and redevelopment activities occur and that the vision and goals for the downtown are achieved through appropriate architectural styles and details.

GOALS

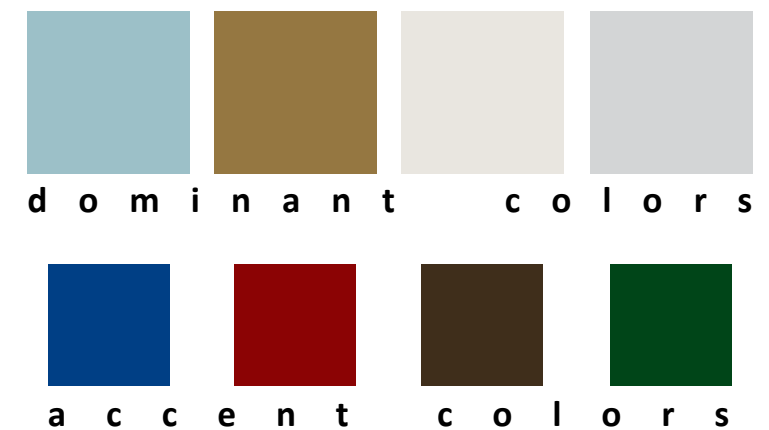
ARCHITECTURE



Through a site site inventory and analysis several parameters were set for general character, architecture and color for the historic downtown. The images above demonstrate the architectural style, color and massing that shall dominate the downtown streetscapes. The main goal is to use common elements (i.e. color) throughout downtown to unit the users and create a more uniform frontage. The following Guidelines shall apply to the Historic Downtown Norwood.

Rich colors with muted tones should be used on structures throughout the historic downtown. Dominant colors on structures should be muted rich tones. For example, light blue, beige, light grey, and chestnut. Non-dominant colors could be more diverse, but should not be more than 20-percent of a buildingfront. Primary colors may be used as accents provided they are consistant with and do not exceed 20-percent of the facade.

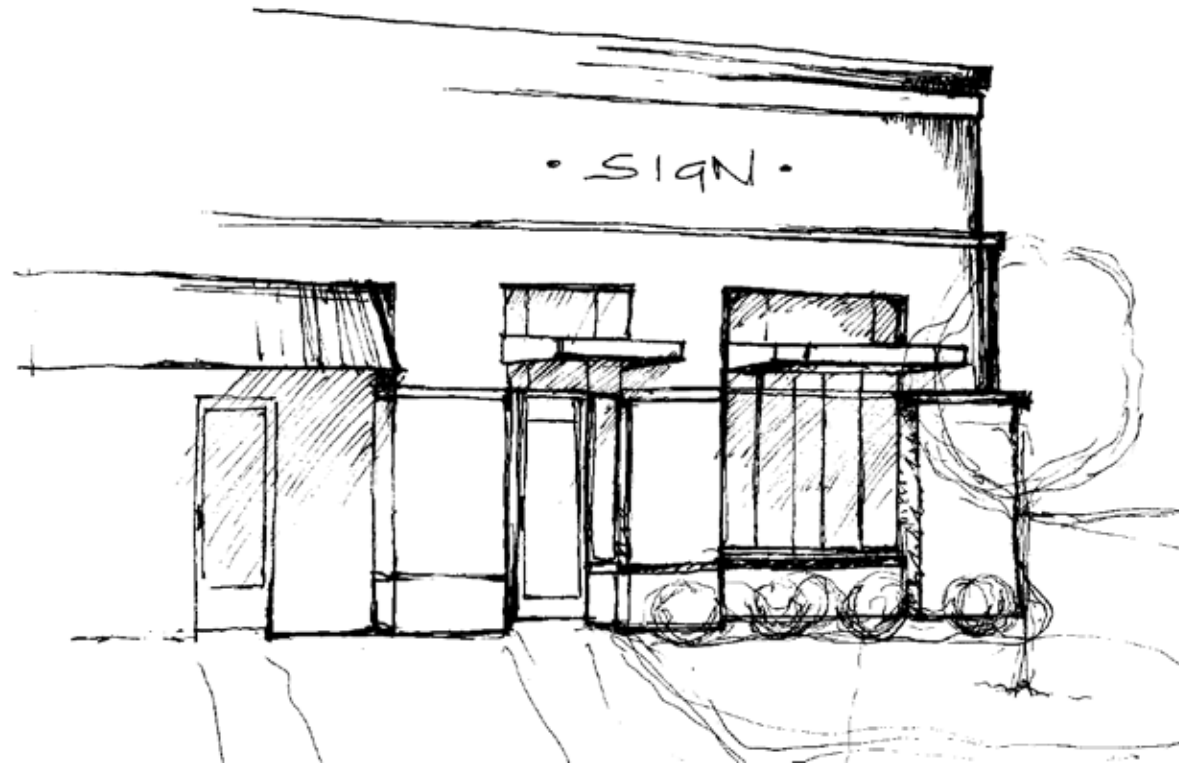
COLOR



ARCHITECTURAL ELEMENTS

ARCHITECTURE - ELEMENTS & CHARACTERISTICS

The sketch and pictures identify the appropriate style of architecture for the historic downtown of Norwood. The downtown architecture should create a special and unique environment and should feel different than the surrounding environment.



The sketch and example pictures demonstrate a mix of architecture styles, including a neo-traditional structure as well as a stick-built traditional structure. All images include the following characteristics that unite them and make them appropriate for Downtown:

- High percentage of glass at the street level makes them inviting and adds an element of transparency.
- Architectural details including the parapets, cornices, window casings and sashes, and roof lines all have clean lines.
- Clearly identified structures and signage affixed to the facade of the structure is required on all buildings in the Downtown District.



ARCHITECTURAL ELEMENTS

Historic Downtown Norwood

ARCHITECTURE

Objective: To create an environment with an architectural palette that is cohesive and unique to historic downtown Norwood.

Individual storefronts and separation between structures (perceived or literal) should dominate the downtown. Mixed-use structures, when stacked, are appropriate for the downtown provided the scales does not exceed two and a half stories.

The stick-build Scandinavian and/or Danish architectural style shall dominate the downtown. Neo-traditional structures are appropriate provided color and scale matches the downtown character. If architectural styles of adjacent structures are different, common elements such as window style or color should be used to help create a more cohesive street frontage.

Buildings and structures shall have a distinct and varying roof line to encourage interest and variety. Structures shall have a district entry to clearly delineate the entrance users.

The first level of all structures shall have a transparent facade that is interesting and inviting.

Four-sided architecture is required particularly for structures with double frontage (i.e. corner lot), or visibility from major thoroughfares.



ARCHITECTURAL ELEMENTS

MATERIALS

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The following material breakdown is required for all structures:

Street Level (Calculation includes parapet and false fronts for single-story structures):

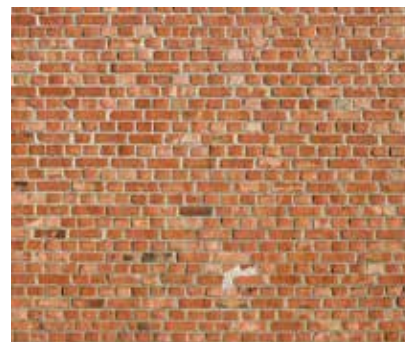
Fenestration/Windows:	60% Minimum
Wood/Brick/Stone/Hardie:	35% Minimum
EIFS/Aluminum/Other:	5% Maximum

2nd Level (Calculation includes parapets and false fronts):

Fenestration/Windows:	40% Minimum
Wood/Brick/Stone/Hardie:	50% Minimum
EIFS/Aluminum/Other:	10% Maximum

The residential scale should be compatible with the scale and massing of adjacent commercial and/or office uses. All residential areas are open to discussion for material breakdown recommendations.

Clearly marked, welcoming entrances shall be required for all retail and office space in the Downtown District. All doors shall have a minimum of 50% of their area transparent (i.e. glass).



ARCHITECTURAL ELEMENTS

SCALE & MASSING

Architectural style and quality will directly influence scale and massing because style and height restrictions will naturally control the size and location of a structure. Additional parameters as set forth below shall also guide development in Downtown.

There should be no more than half (½) story difference between structures on the street to create some visual consistency.

Parapets and false fronts may be used to achieve greater consistency along the street frontage.

Parapets and false fronts will be included in materials calculations on the façade. These areas must include architectural detail, and/or fenestration to add visual interest to a façade.

Building space can be literal or perceived. (See Image) Buildings may be connected, but the illusion of spacing must be created to eliminate the appearance of a strip mall or connected multi-use structures.

Architectural detailing and fenestration should be used to reduce large expanses of walls without interest.

Buildings shall be a minimum of 15-feet wide but should be no larger than 40-feet wide; each user must have a minimum of 15-feet of frontage at the street-level.

Structures shall be no taller than 2.5 stories (35-feet maximum), where the half story is attributed to a parapet, false front, or roof pitch.



The above images represent the existing and appropriate scale and massing for the historic downtown of Young America. Parapets, false fronts and facade treatment help to break up the mass of structures and to create a quaint downtown feel.

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SIGNAGE

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All proposed signage should meet the current sign ordinance. In addition to these recommendations, the following standards shall be applied to signs within the Downtown of Norwood.

Clean, elegant signage with small town character. Materials and scale should reflect the materials and scale of each user.

Supplemental signs are acceptable in this District, including sandwich boards and projecting signs.

Wall signage (i.e. signage affixed to the structure and flush with the façade) is required to ensure visibility of each user.

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- **Lighting:** All signs within the Downtown District must have secondary lighting. Internal lighting is not acceptable. This includes neon signs which are also not permitted. Backlit signs are appropriate provided they meet lighting standards set forth in the City's zoning code. Light pollution should be reduced when possible and energy reducing light fixtures used when possible.

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ARCHITECTURAL ELEMENTS

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Off-street parking lot areas and vacant parcels shall be reduced and infilled wherever possible. Off-street parking lot widths shall be encouraged to reduce drive aisles to one per parking lot with two parking stall aisles on either side. Wherever an off-street parking lot or vacant parcel of land is to remain, permanent ornamental fencing and/or vertical landscaping shall be implemented at the front perimeter to create a clearly defined street frontage and pedestrian walkway in the downtown district.



The off-street parking lot and vacant parcel of land shown above demonstrate examples of land that may be infilled or implemented with permanent ornamental fencing and/or vertical landscaping at the street frontage perimeters to clearly define appropriate setbacks for the downtown district.



ARCHITECTURAL ELEMENTS

IMPLEMENTATION OF DESIGN GUIDELINES

The implementation of these standards is critical to the overall success of these Guidelines. As stated previously, these standards are Guidelines and are meant to help future builders and developers understand the goals and aspirations with respect to design in the community. The City is committed to creating an identity and strong sense of place through design. The City does not want to limit creativity or unique development that might have a place in the community, which is why these Guidelines are designed to be general and broad, therefore creating opportunities for great design.

Although creativity and individuality is encouraged, these Guidelines must be enforced as a minimum standard throughout each District. The images and sketches throughout the document visually represent the character for the community. These images are not literal interpretations of the Historic Downtowns of Norwood and Young America, but they do suggest the direction of future design for the community.



Implementation of these Guidelines will occur in several ways and will affect a builder and developer at a variety of different times throughout the planning and design process. First and foremost, zoning and land use plays a role in these Guidelines and it is the responsibility of the developer or builder to understand how these regulations and the Guidelines work together.





IMPLEMENTATION

Historic Downtown of Norwood & Young America

Exhibit C – Sample Historic Property Form







Property History

Address: _____

Year Constructed (if known): _____

Historical Photo of Property

Date of Photo: _____
Historical Uses: _____

Current Photo of Property

Date of Photo: _____
Current Use: _____
Current Owner: _____

Other Information (such as past owners, unique history, years modified, etc.): _____

